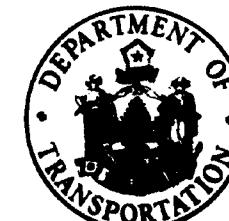


STATE OF MAINE
DEPARTMENT OF TRANSPORTATION



PLANS

BRIDGE WEARING SURFACE
REPLACEMENTS

PROJECT NO. IR-95-7 (96)

I-95 NB over SEBASTICOOK RIVER (5990)
I-95 SB over SEBASTICOOK RIVER (1446)
I-95 NB over NORTH MAIN STREET (5989)
I-95 SB over NORTH MAIN STREET (1445)
I-95 NB over WEBB ROAD (5984)
I-95 SB over WEBB ROAD (1449)
I-95 NB over SOMERSET AVENUE (5985)
I-95 SB over SOMERSET AVENUE (1447)
I-95 NB over MAINE CENTRAL R.R. (5988)
I-95 NB over ROUTE 152 (5987)
I-95 SB over ROUTE 152 & MAINE CENTRAL R.R. (5986)

IN THE TOWN OF
PITTSFIELD
SOMERSET COUNTY

| CONVENTIONAL SIGNS | |
|---------------------------------|-------|
| COUNTY LINES | ----- |
| TOWN LINES | ----- |
| PROPERTY LINES | ----- |
| R/W LINES-EXISTING | ===== |
| R/W LINES-NEW-ACCESS CONTROL | ===== |
| R/W LINES-NEW-NO ACCESS CONTROL | ===== |
| CULVERT-EXISTING | ===== |
| CULVERT-PROPOSED | ===== |
| CURBING-EXISTING | ===== |
| CURBING-PROPOSED | ===== |
| TRAVELLED WAY-EXISTING | ===== |
| TRAVELLED WAY-PROPOSED | ===== |
| UNDERGROUND UTILITIES-EXISTING | ----- |
| UNDERGROUND UTILITIES-PROPOSED | ----- |
| RAILROAD-SINGLE TRACK | ===== |
| RAILROAD-DOUBLE TRACK | ===== |
| UTILITY POLE-EXISTING | ===== |
| UTILITY POLE-JOINT OCCUPANCY | ===== |
| PROPOSED UTILITY POLE-TEMPORARY | ===== |
| PROPOSED UTILITY POLE-PERMANENT | ===== |
| TREES | ===== |
| WOODS | ===== |

INDEX OF SHEETS
SHEET DESCRIPTION

1. Title Sheet
2. Estimated Quantities
3. General Plans & Sections
4. General Plans
5. Sections
6. Joint Details
7. End Post & Guard Rail Connection Detail
8. Reinforcing Steel Schedule
9. BD 125-82 Expansion Device
10. BD 127-81 Miscellaneous Details (Barriers)
11. (HD-6) Type 3 Guard Rail (Terminal Connector)
- 12 thru 14. Traffic Control Plans

SPECIFICATIONS

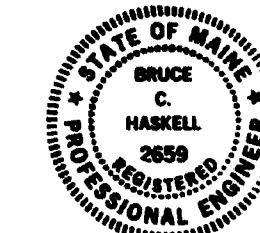
DESIGN: LOAD FACTOR DESIGN PER AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 1983, AND INTERIM SPECIFICATIONS THROUGH 1985.

CONTRACT: STATE OF MAINE, DEPARTMENT OF TRANSPORTATION, STANDARD SPECIFICATIONS HIGHWAYS AND BRIDGES, REVISION OF JAN. 1984.

MATERIALS: CONCRETE--CLASS A-A
REINFORCING STEEL--ASTM A615, GRADE 60.

BASIC DESIGN STRESSES: CONCRETE-- $f'_c = 3,000$ PSI;
REINFORCING STEEL-- $f_y = 60,000$ PSI.

PLANS OF THE EXISTING BRIDGES ARE AVAILABLE FOR THE CONTRACTOR'S REFERENCE AT THE BRIDGE DESIGN OFFICE IN AUGUSTA. THE PLANS ARE REPRODUCTIONS OF ORIGINAL DRAWINGS AS PREPARED FOR THE CONSTRUCTION OF THE BRIDGES AND IT IS VERY UNLIKELY THAT THE PLANS WILL SHOW ANY CONSTRUCTION FIELD CHANGES OR ANY ALTERATIONS WHICH MAY HAVE BEEN MADE TO THE BRIDGES DURING THEIR LIFE SPAN.



CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE

UNITED STATES
DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION 1

APPROVED:

DIVISION ADMINISTRATOR DATE

APPROVED:

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
COMMISSIONER

DATE

June 13, 1989

Richard Coleman
CHIEF ENGINEER

June 13, 1989

99-459

4.6 to 5.1 p.m. 1/1/89

NOTE

All work contemplated under this contract to be governed by and in conformity with the STANDARD SPECIFICATIONS (revision of January 1984) and supplementals thereto, except as modified on the plans and in the special provisions.

| ESTIMATED QUANTITIES | | TOTAL | | S.B. OVER | N.B. OVER | S.B. OVER | N.B. OVER | S.B. OVER | N.B. OVER | S.B. OVER | N.B. OVER | S.B. OVER | N.B. OVER | S.B. OVER | N.B. OVER |
|--------------------------------------|---|----------|------|-------------------|-------------------|----------------|----------------|-----------|-----------|-----------------|-----------------|--------------------|--------------------|--------------------|--------------------|
| ITEM NO. | DESCRIPTION | QUANTITY | UNIT | SEBASTICOOK RIVER | SEBASTICOOK RIVER | N. MAIN STREET | N. MAIN STREET | WEBB ROAD | WEBB ROAD | SOMERSET AVENUE | SOMERSET AVENUE | M.C.R.R. ROUTE 152 | M.C.R.R. ROUTE 152 | M.C.R.R. ROUTE 152 | M.C.R.R. ROUTE 152 |
| 202.127 | REMOVAL OF EXISTING BITUMINOUS PAVEMENT | 1 | L.S. | 0.12 | 0.12 | 0.07 | 0.07 | 0.07 | 0.07 | 0.08 | 0.08 | 0.17 | 0.08 | 0.07 | |
| 202.202 | Removing Pavement Surface | 7377 | S.Y. | 683 | 683 | 683 | 683 | 683 | 683 | 683 | 683 | 683 | 615 | 615 | |
| 403.10 | HOT BITUMINOUS PAVEMENT GRADING D | 1898 | TONS | 210 | 210 | 146 | 146 | 146 | 146 | 151 | 151 | 286 | 155 | 151 | |
| 403.121 | HOT BITUMINOUS PAVEMENT GRADING E (SHIMMING) | 108 | TONS | 10 | 10 | 10 | 10 | 10 | 10 | 10 | 10 | 10 | 9 | 9 | |
| 410.15 | Emlulified Asphalt Applied | 736 | G | 68 | 68 | 68 | 68 | 68 | 68 | 68 | 68 | 68 | 62 | 62 | |
| 503.12 | Reinforcing Steel, Fabricated and Delivered | 5448 | Lbs. | 503 | 503 | 503 | 503 | 503 | 503 | 503 | 503 | 418 | 503 | 503 | |
| 503.13 | Reinforcing Steel, Placing | 5448 | Lbs. | 503 | 503 | 503 | 503 | 503 | 503 | 503 | 503 | 418 | 503 | 503 | |
| 506.142 | FIELD PAINT EXISTING STRUCTURAL STEEL | 1 | L.S. | 0.15 | 0.15 | 0.06 | 0.06 | 0.06 | 0.06 | 0.07 | 0.07 | 0.17 | 0.08 | 0.07 | |
| 508.13 | MEMBRANE WATERPROOFING | 1 | L.S. | 0.11 | 0.11 | 0.08 | 0.08 | 0.08 | 0.08 | 0.08 | 0.08 | 0.14 | 0.08 | 0.08 | |
| 514.06 | CURING BOX FOR CONCRETE CYLINDERS | 1 | EACH | | | | | | | | | | | | |
| 518.30 | REHAB. OF STRUCTURAL CON. SLAB-TO-REINFORCING STEEL | 5625 | S.F. | 445 | 445 | 217 | 163 | 162 | 162 | 229 | 229 | 1310 | 929 | 890 | |
| 518.31 | REHAB. OF STRUCTURAL CON. SLAB-TO-BELOW REINF. STEEL | 2655 | S.F. | 267 | 445 | 108 | 0 | 0 | 0 | 114 | 114 | 393 | 619 | 595 | |
| 520.2401 | BRIDGE JOINT MODIFICATION | 10 | EA. | 1 | 1 | 1 | 2 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | |
| 520.2402 | BRIDGE JOINT MODIFICATION | 5 | EA. | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | |
| 520.2403 | Bridge Joint Modification | 6 | EA. | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | |
| 526.301 | Temporary Concrete Barrier, Type 1 | 1 | L.S. | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.05 | 0.05 | |
| 606.173 | Bridge Connections | 22 | EA. | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | |
| 627.611 | 6 inch Solid White Pavement Marking Line | 3,600 | L.F. | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 180 | 180 | |
| 627.621 | 6 inch Broken White Pavement Marking Line | 3,600 | L.F. | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 180 | 180 | |
| 627.631 | 6 inch Solid Yellow Pavement Marking Line | 3,600 | L.F. | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 180 | 180 | |
| 627.67 | Removing Pavement Markings | 1,600 | S.F. | 160 | 160 | 160 | 160 | 160 | 160 | 160 | 160 | 160 | 80 | 80 | |
| 627.681 | Temporary 6 inch Painted Pavement Marking Line, Yellow or White | 3,600 | L.F. | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 360 | 180 | 180 | |
| 627.69 | Temporary 4 inch Plastic Pavement Marking Line, Yellow or White | 1,600 | L.F. | 160 | 160 | 160 | 160 | 160 | 160 | 160 | 160 | 160 | 80 | 80 | |
| 638.20 | Field Office Type C | 1 | EA. | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.05 | 0.05 | |
| 652.30 | Flashing Arrow Board | 2 | EA. | 0.20 | 0.20 | 0.20 | 0.20 | 0.20 | 0.20 | 0.20 | 0.20 | 0.20 | 0.10 | 0.10 | |
| 652.31 | Type 1 Barricade | 100 | EA. | 10 | 10 | 10 | 10 | 10 | 10 | 10 | 10 | 10 | 5 | 5 | |
| 652.33 | Drum | 20 | EA. | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 1 | 1 | |
| 652.34 | Cone | 20 | EA. | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 1 | 1 | |
| 652.35 | Construction Signs | 700 | S.F. | 70 | 70 | 70 | 70 | 70 | 70 | 70 | 70 | 70 | 35 | 35 | |
| 652.361 | Maintenance of Traffic Control Devices | 1 | L.S. | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.05 | 0.05 | |
| 652.38 | Flagger | 500 | M.H. | 50 | 50 | 50 | 50 | 50 | 50 | 50 | 50 | 50 | 25 | 25 | |
| 659.10 | Mobilization | 1 | L.S. | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.10 | 0.05 | 0.05 | |
| = BREAKDOWN OF LUMP SUM QUANTITIES = | | | | | | | | | | | | | | | |
| 202.127 | REMOVAL OF EXISTING BITUMINOUS PAVEMENT | 8420 | S.Y. | 987 | 987 | 599 | 599 | 598 | 598 | 631 | 631 | 1442 | 683 | 660 | |
| 506.142 | FIELD PAINT EXISTING STRUCTURAL STEEL | 1234500 | LB. | 287800 | 287800 | 115350 | 115350 | 113000 | 113000 | 132500 | 132500 | 355600 | 147800 | 133800 | |
| 508.13 | MEMBRANE WATERPROOFING | 8420 | S.Y. | 987 | 987 | 599 | 599 | 598 | 598 | 631 | 631 | 1442 | 683 | 660 | |

| F.R.A. | STATE | PROJECT NUMBER | SHEET | TOTAL |
|--------|-------|----------------|-------|-------|
| 1 | MAINE | IR-95-7(96) | 2 | 14 |

CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

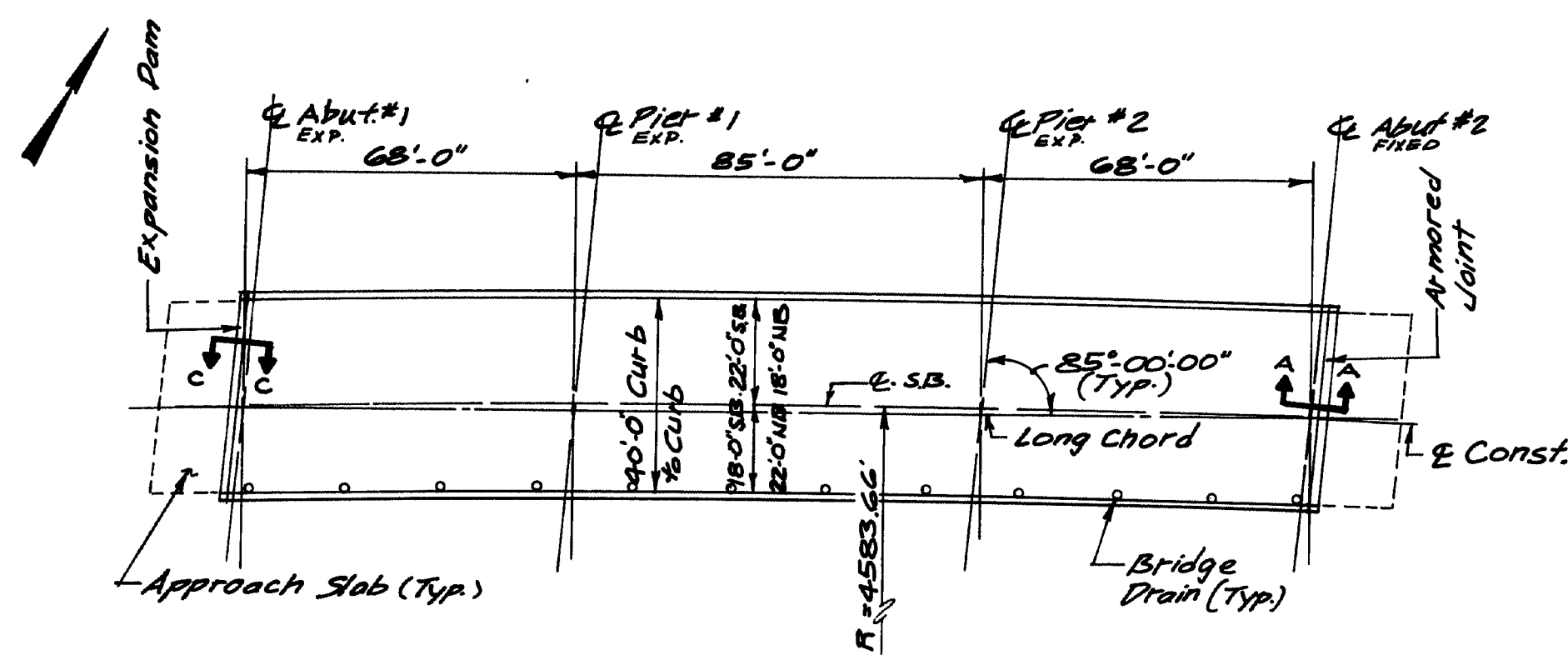
PITTSFIELD
ESTIMATED QUANTITIES

99-460
A. b. + Dec 1999
Kpm

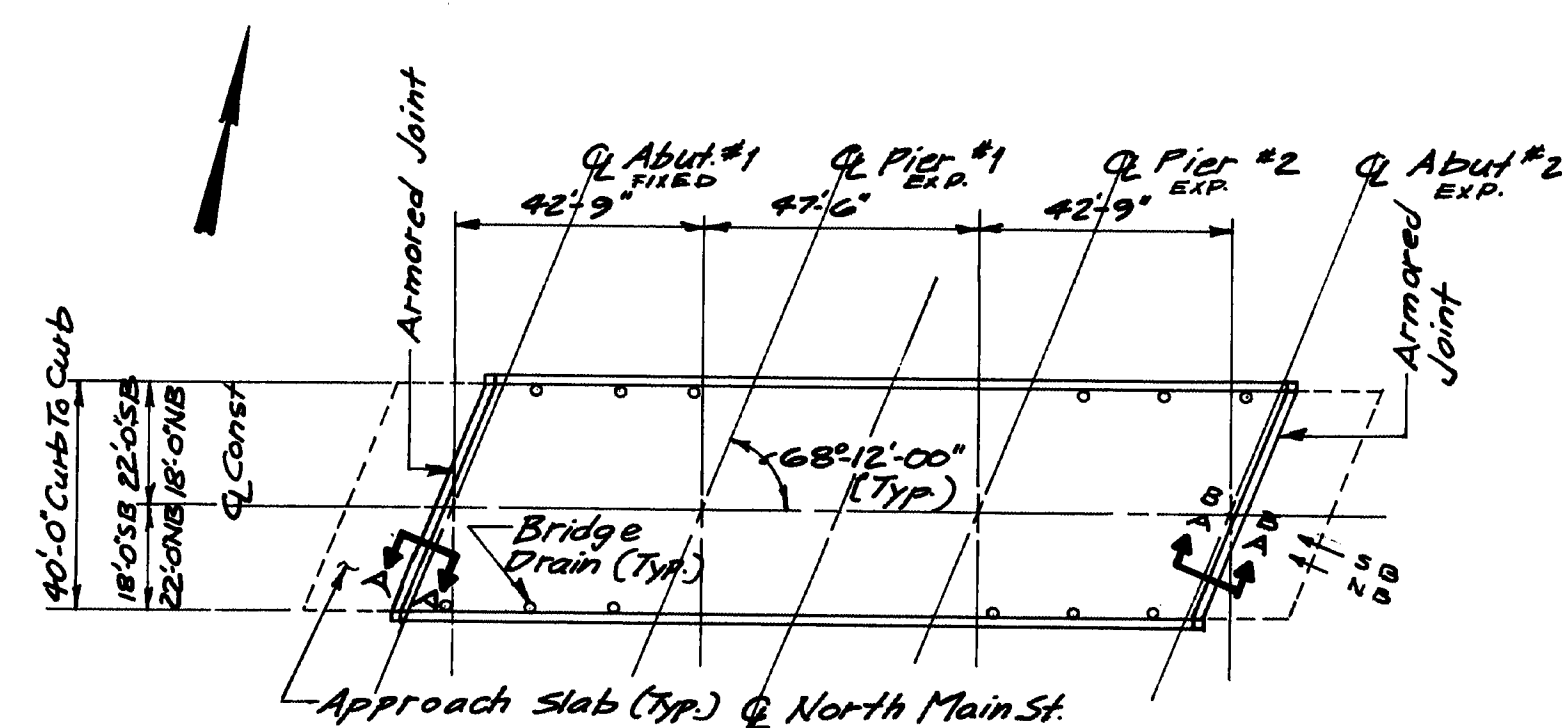
PROJECT DESIGN ENGINEER
DESIGN-DETAILED
CHECKED
REVISIONS
FIELD CHANGES
PLANS
DATE
BY
DAY
MONTH
YEAR

PRINTING 44-132 20710-1

| F.H. No. | STATE | PROJECT NUMBER | SHEET NO. | TOTAL SHEETS |
|----------|-------|----------------|-----------|--------------|
| 1 | MAINE | IR-95-7(14) | 3 | 14 |



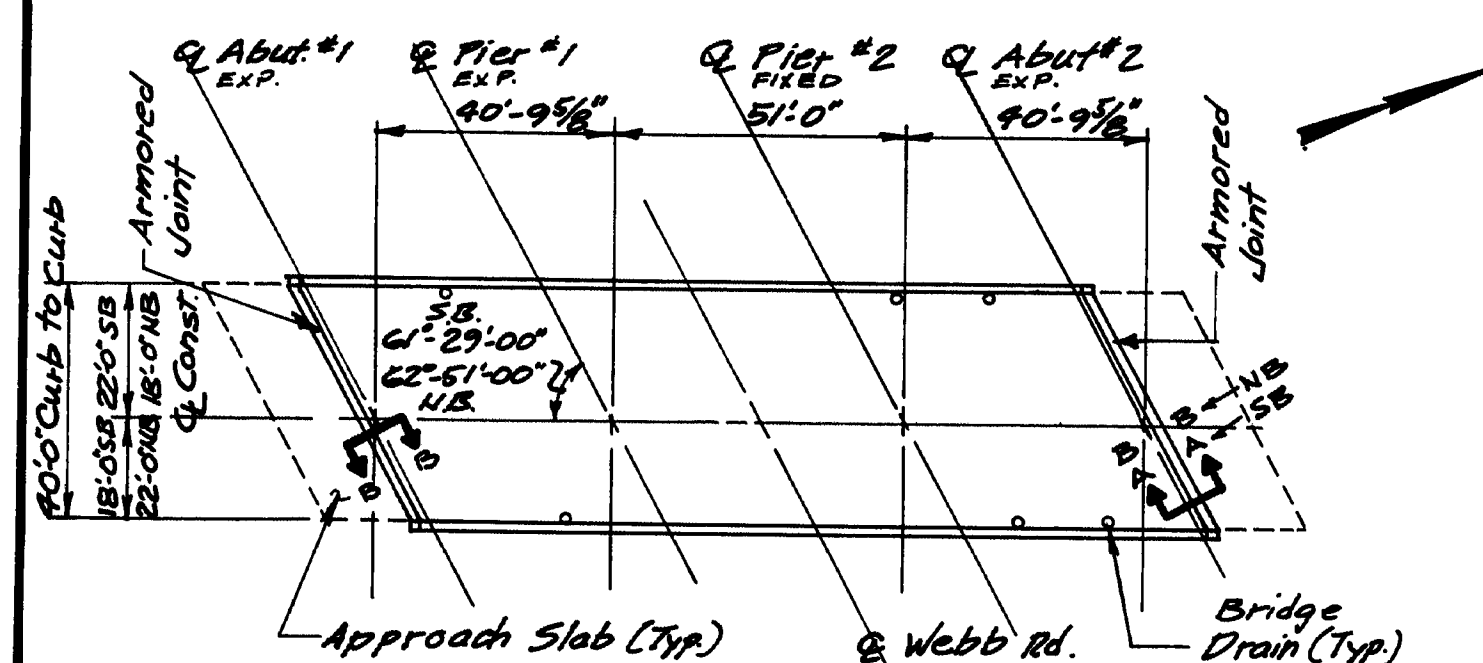
I-95 SOUTHBOUND AND NORTHBOUND OVER SEBASTICOOK RIVER 5990 N.B. 1446 S.B.



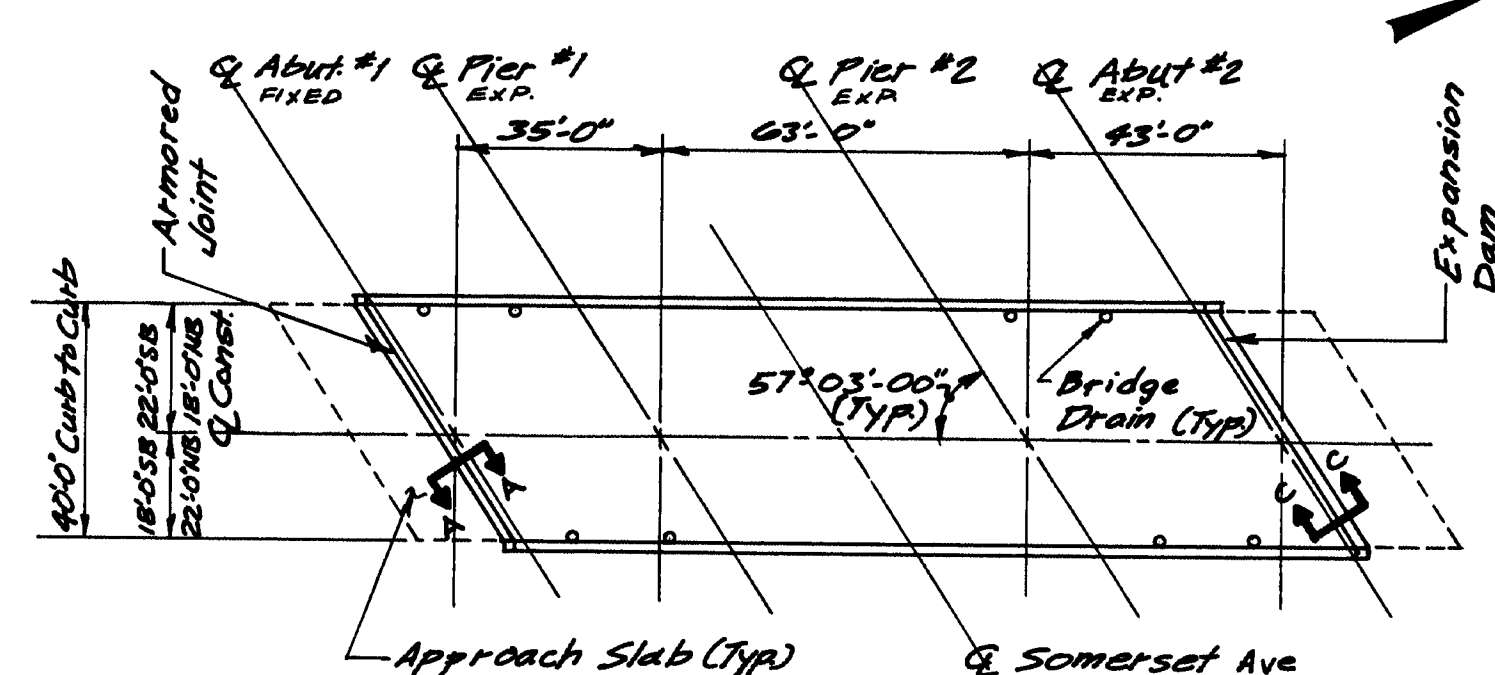
I-95 SOUTHBOUND AND NORTHBOUND OVER NORTH MAIN STREET 5989 N.B. 1445 S.B.

SCOPE OF WORK

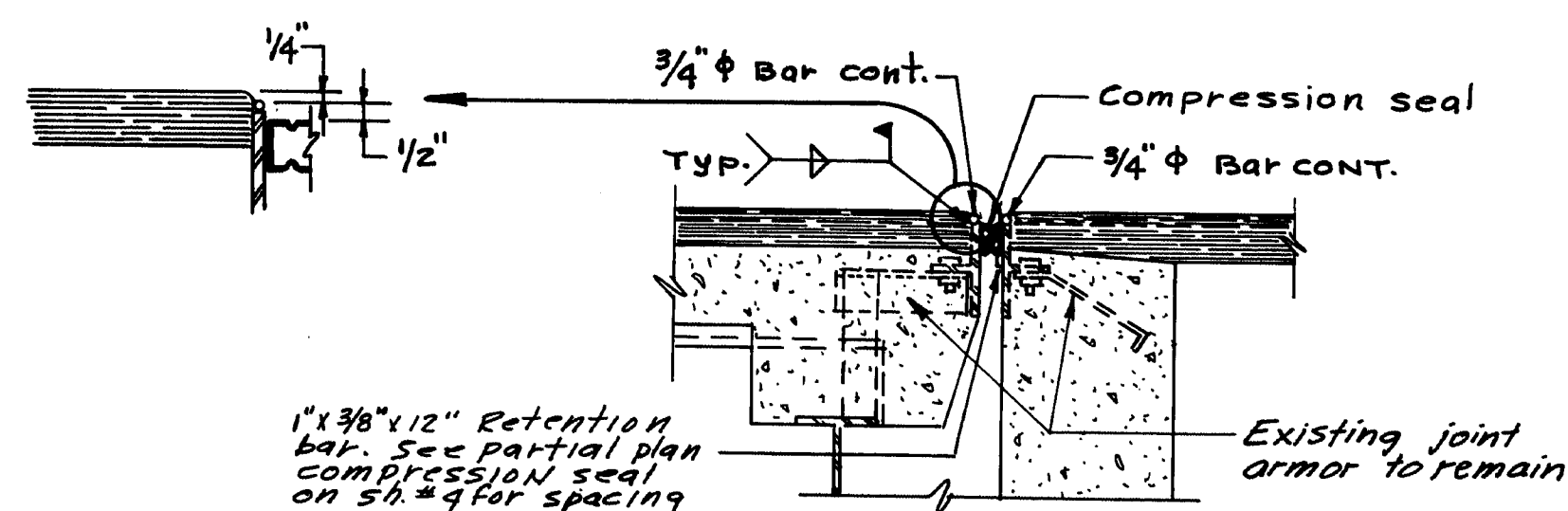
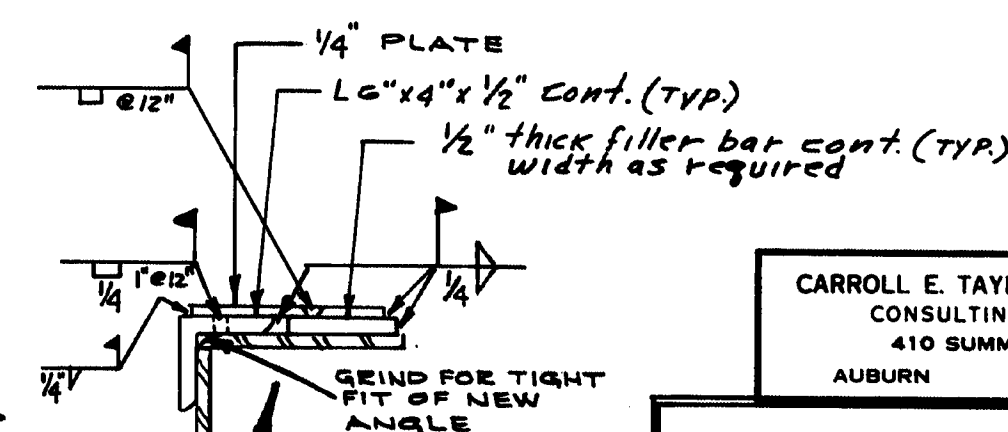
- ALL BRIDGES:
 REMOVE 2-INCH BITUMINOUS WEARING SURFACE AND REMOVE EXISTING MEMBRANE WATER-PROOFING.
 REPAIR DECK AS NECESSARY.
 MODIFY EXPANSION JOINTS *where indicated*.
 INSTALL 3-INCH BITUMINOUS WEARING SURFACE PLUS MEMBRANE *waterproofing*.
 CLEAN AND PAINT ALL STRUCTURAL STEEL.
 MODIFY APPROACH PAVEMENT FOR NEW GRADES.



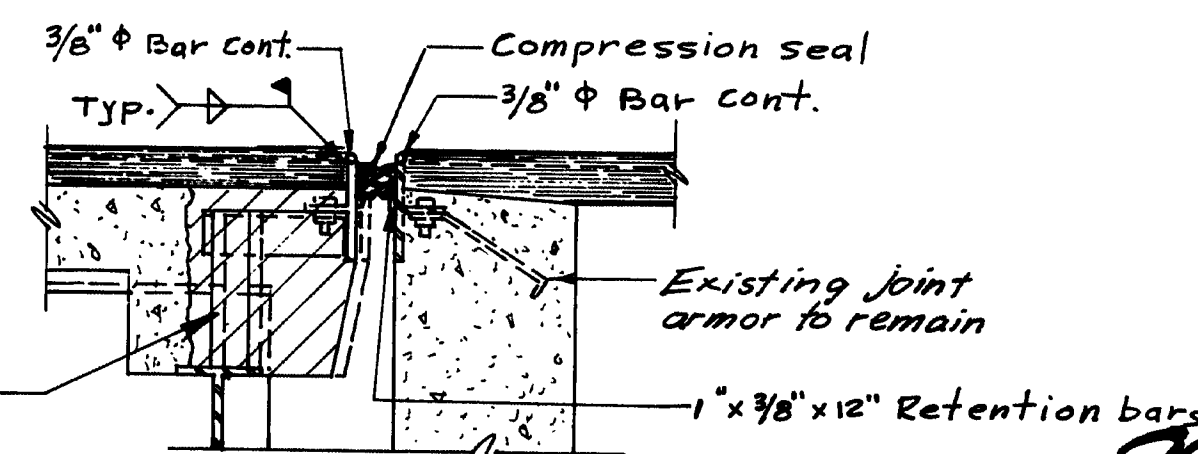
I-95 SOUTHBOUND AND NORTHBOUND OVER WEBB ROAD 5984 N.B. 1449 S.B.



I-95 SOUTHBOUND AND NORTHBOUND OVER SOMERSET AVENUE 5985 N.B. 1447 S.B.

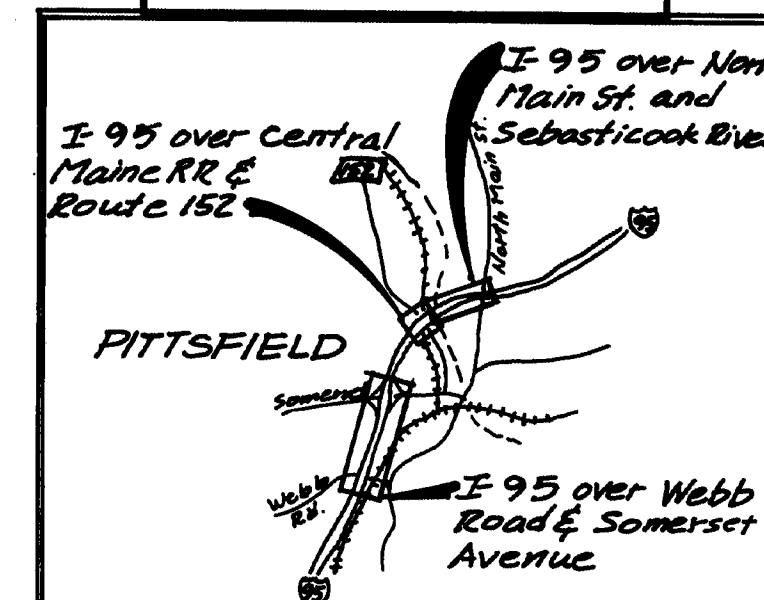


SECTION A-A



SECTION B-B

SECTION C-C



LOCATION MAP

0 1 2 3 4
Scale in miles

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95
SOUTHBOUND AND NORTHBOUND
OVER

Sebasticook River, North Main
Street, Webb Road, Somerset
Avenue, Central Maine RR & Route 152
PITTSFIELD

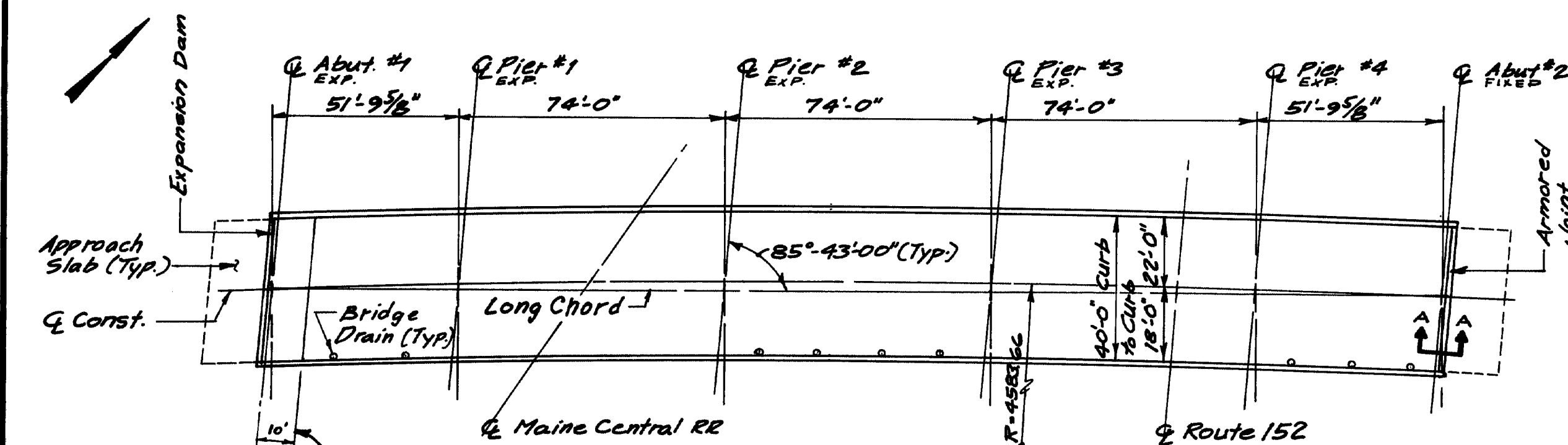
GENERAL PLAN & SECTIONS

SHEET 3 OF 14 AUGUSTA, MAINE

| PROJECT ENGINEER | BY | DATE |
|------------------|--------|--------|
| DESIGN - DETAIL | DAY | 2.2.11 |
| CHECKED | B.E.H. | |
| REVISIONS | | |
| FIELD CHANGES | | |

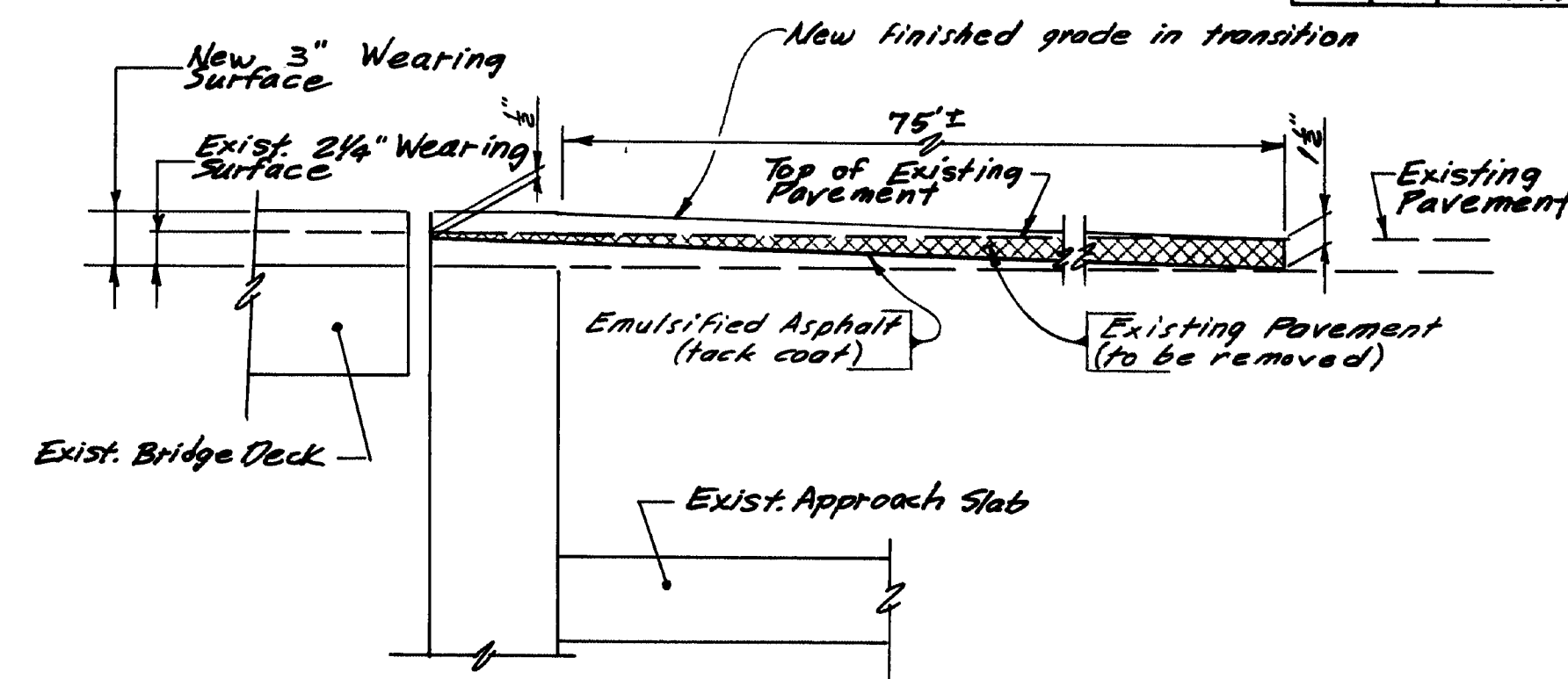
99-461
As bu 14 Dec 1999

| F.R. NO. | STATE | PROJECT NUMBER | SHEET NO. | TOTAL SHEETS |
|----------|-------|----------------|-----------|--------------|
| 1 | MAINE | IR-95-7(96) | 4 | 14 |

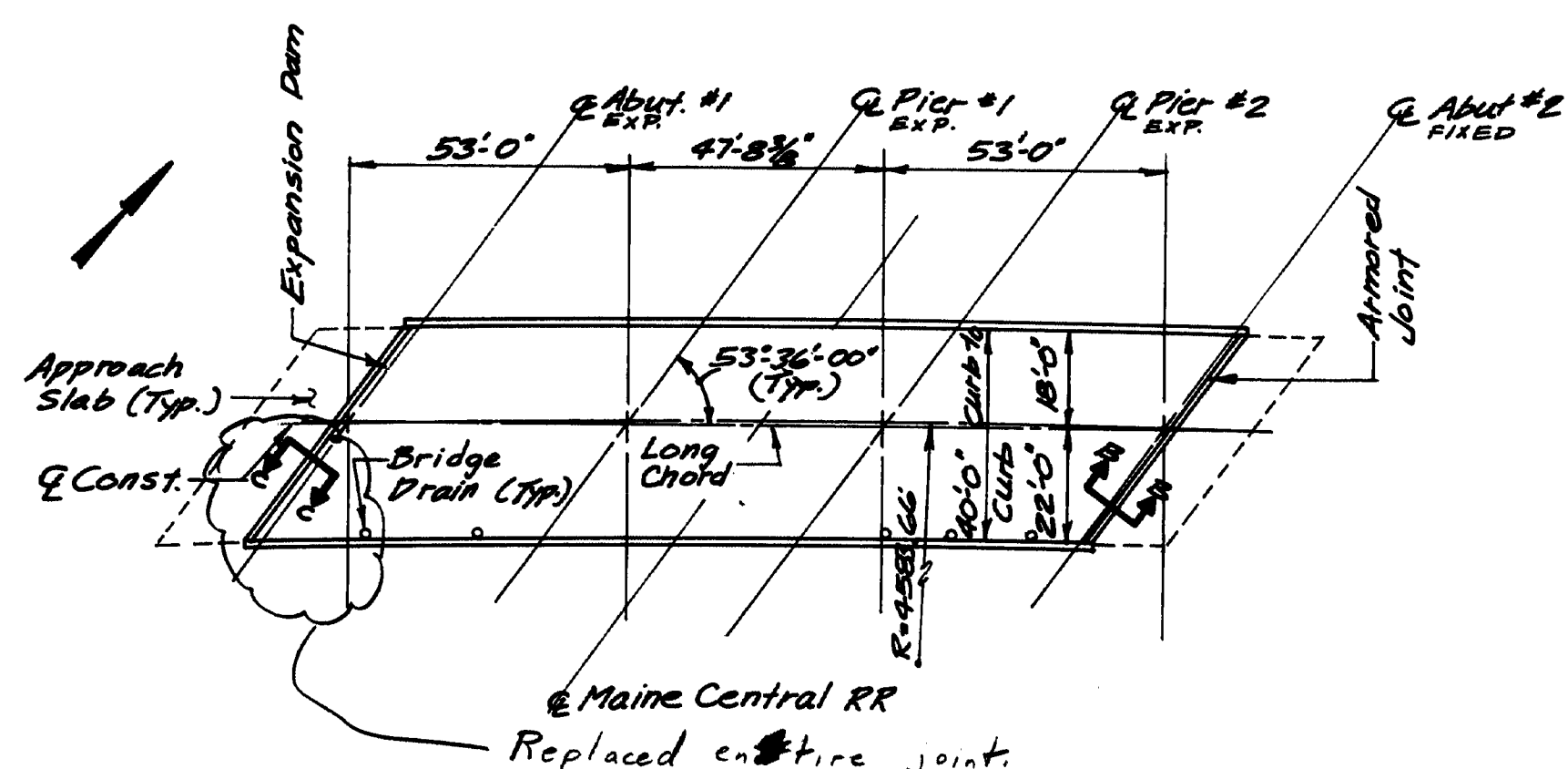


TRANSITION NEW PAVEMENT THICKNESS TO MATCH EXIST. FINGER JOINT HEIGHT

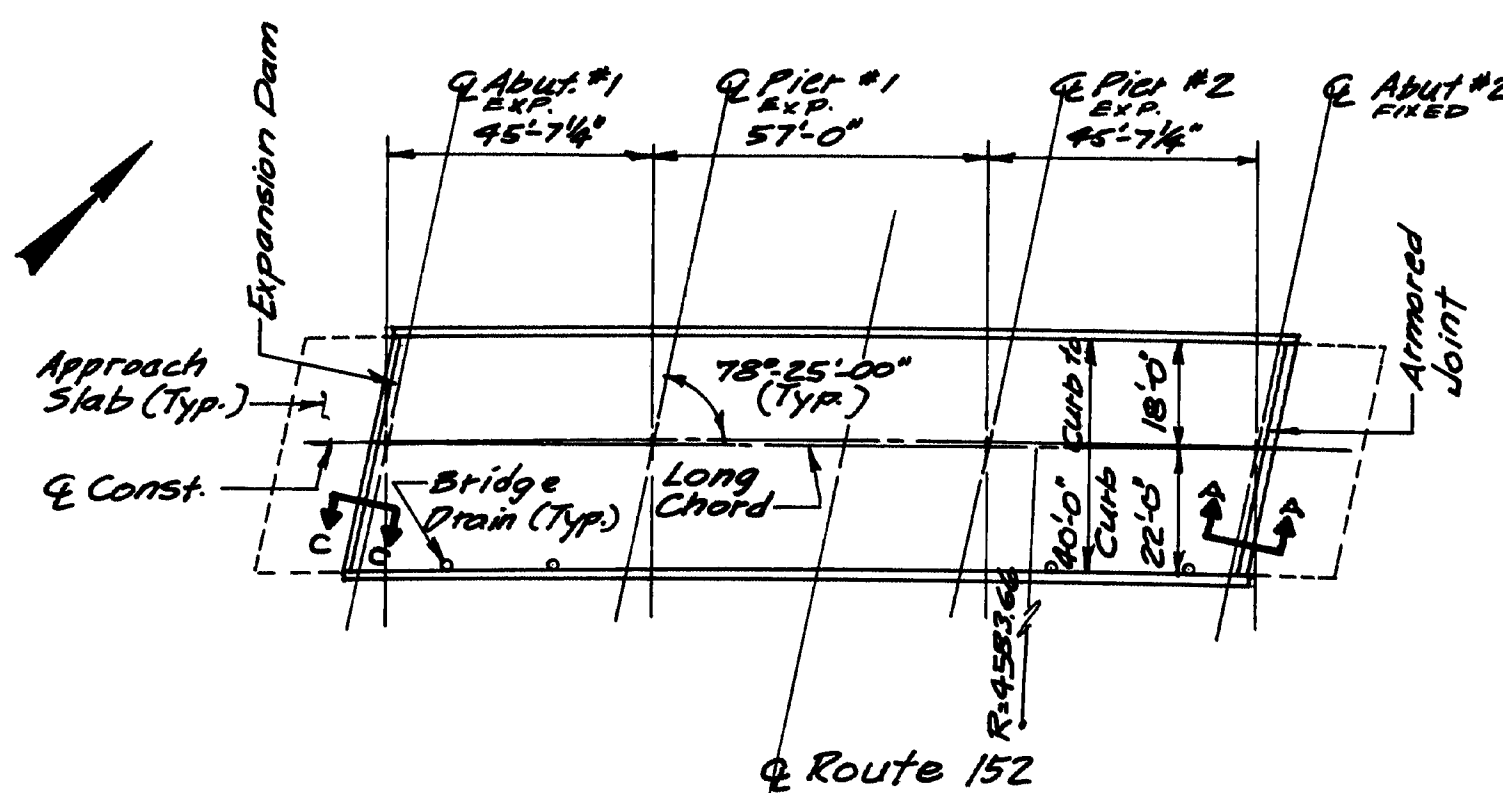
I-95 SOUTHBOUND OVER MAINE CENTRAL RR AND ROUTE 152 5986



APPROACH PAVEMENT TRANSITION
(Typical all bridges, Each end.)



I-95 NORTHBOUND OVER MAINE CENTRAL RR 5988



I-95 NORTHBOUND OVER ROUTE 152 5987

CONSTRUCTION NOTES

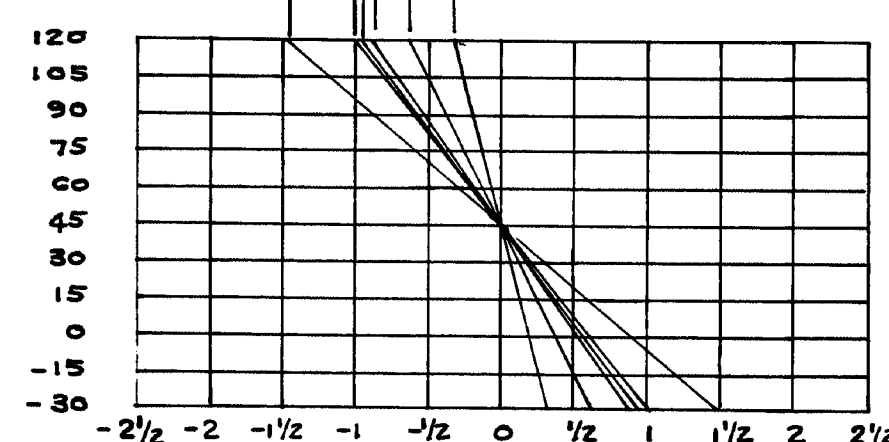
1. MAINTAIN ONE 1/3-FOOT MINIMUM TRAFFIC LANE AT ALL TIMES.
2. ALL WORK SHALL BE DONE BEHIND TEMPORARY CONCRETE BARRIERS.
3. THE TOP SURFACE OF THE EXISTING CONCRETE SLABS SHALL BE REPAIRED AS DIRECTED BY THE ENGINEER.
4. DEPRESS THE BITUMINOUS WEARING SURFACE AROUND THE EXISTING BRIDGE DRAINS AS DIRECTED BY THE ENGINEER.
5. BECAUSE OF STAGED CONSTRUCTION, SOME EXPANSION JOINTS MAY REQUIRE CONSTRUCTION JOINTS. THESE SHALL BE AS APPROVED BY THE ENGINEER AS TO TYPE AND LOCATIONS.
6. PAYMENT FOR DRILLING AND GROUTING ASSOCIATED WITH MODIFICATIONS OF THE JOINTS WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.
7. PAYMENT FOR REMOVING AND RE-INSTALLING BRIDGE RAIL OR GUARDRAIL, AS NEEDED TO ACCOMPLISH JOINT MODIFICATIONS, WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.
8. PAYMENT FOR REMOVING AND REPLACING PORTIONS OF END POSTS (CONCRETE PARAPET AND CURB) WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.
9. PAYMENT FOR CUTTING, REMOVING, OR REPLACING GRANITE CURB WHERE CALLED FOR WILL BE CONSIDERED INCIDENTAL TO THE BRIDGE JOINT MODIFICATION ITEMS.
10. REINFORCING STEEL SHALL HAVE A MINIMUM COVER OF 2 INCHES UNLESS NOTED OTHERWISE.
11. WHERE GRANITE CURB IS CALLED FOR TO BE REMOVED TO MODIFY A JOINT OR TO INSTALL A SEAL, ONLY THE SMALLEST AMOUNT NECESSARY TO ACCOMPLISH THE WORK SHALL BE REMOVED. IT IS PREFERABLE TO REMOVE CURB TO AN EXISTING JOINT BUT IF A JOINT IS MORE THAN 3 FEET AWAY THE CURB SHALL BE SAW CUT IN THE FIELD. THE DECISION ABOUT WHETHER TO CUT OR NOT, AND WHERE, SHALL BE THE ENGINEER'S.
12. SEE STANDARD DETAIL SH 80125-02 FOR ADDITIONAL EXPANSION DEVICE DETAILS.
13. THE REMOVAL OF THE EXISTING BITUMINOUS PAVEMENT ON THE APPROACHES SHALL be paid for under Item 202.202.

1. SEALS TO BE FURNISHED SHALL HAVE A MOVEMENT RATING OF:

- EXIST. - NB/SEBASTICOOK R. (ABUT. #1)
- 2 7/8" - SB/SEBASTICOOK R. (ABUT. #1)
- 2 7/8" - NB/SEBASTICOOK R. (ABUT. #1)
- 2" - SB/SOMERSET AVE. (ABUT. #2)
- 1 7/8" - NB/SOMERSET AVE. (ABUT. #2)
- 1 7/8" - NB/ROUTE 152 (ABUT. #1)
- 1 3/4" - SB/N. MAIN ST. (ABUT. #2)
- 1 3/4" - NB/N. MAIN ST. (ABUT. #2)
- 1 1/4" - SB/WEBB ROAD (ABUT. #1)
- 1 1/4" - NB/WEBB ROAD (ABUT. #1)
- 5/8" - SB/WEBB ROAD (ABUT. #2)
- 5/8" - NB/WEBB ROAD (ABUT. #2)
- EXIST. - SB/SOMERSET AVE. (ABUT. #1)
- EXIST. - SB/SEBASTICOOK R. (ABUT. #2)
- EXIST. - NB/SEBASTICOOK R. (ABUT. #2)
- EXIST. - NB/SOMERSET AVE. (ABUT. #1)
- EXIST. - SB/MCRR & ROUTE 152 (ABUT. #2)
- 5/8" - NB/MCRR (ABUT. #2)
- EXIST. - NB/ROUTE 152 (ABUT. #2)

2. SEALS SHALL BE APPROVED BY THE ENGINEER PRIOR TO FABRICATION OF THE JOINT ARMOR.
3. COMPRESSION SEAL JOINT OPENINGS WILL VARY DEPENDING ON THE DIMENSIONS OF THE SEAL SELECTED BY THE CONTRACTOR. THE JOINT OPENING SHALL BE SET ACCORDING TO THE OPENING SHOWN ON THE APPROVED SHOP DETAIL DRAWINGS.
4. THE SEAL ADJUSTMENT CHART SHOWS THE ADJUSTMENT NECESSARY TO ADJUST THE JOINT OPENING SHOWN ON THE SHOP DETAIL DRAWINGS FOR TEMPERATURES OTHER THAN 45°F. ADJUSTMENT IS TO BE MEASURED PARALLEL TO THE CENTERLINE OF CONSTRUCTION.

- I-95 N.B. & S.B. OVER SOMERSET AVE. - ABUT. 2
- I-95 N.B. OVER ROUTE 152 - ABUT. 1
- I-95 N.B. OVER MCRR - ABUT. 1
- I-95 N.B. & S.B. OVER SEBASTICOOK RIVER
- I-95 S.B. & N.B. OVER N. MAIN STREET
- I-95 S.B. & N.B. OVER WEBB ROAD - ABUT. #1
- I-95 S.B. & N.B. OVER WEBB ROAD - ABUT. #2



SETTING

CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95
SOUTHBOUND AND NORTHBOUND
OVER

Sebasticook River, North Main
Street, Webb Road, Somerset
Avenue, Central Maine RR & Route 152
PITTSFIELD

GENERAL PLANS

SHEET 4 OF 14 AUGUSTA, MAINE

99-462

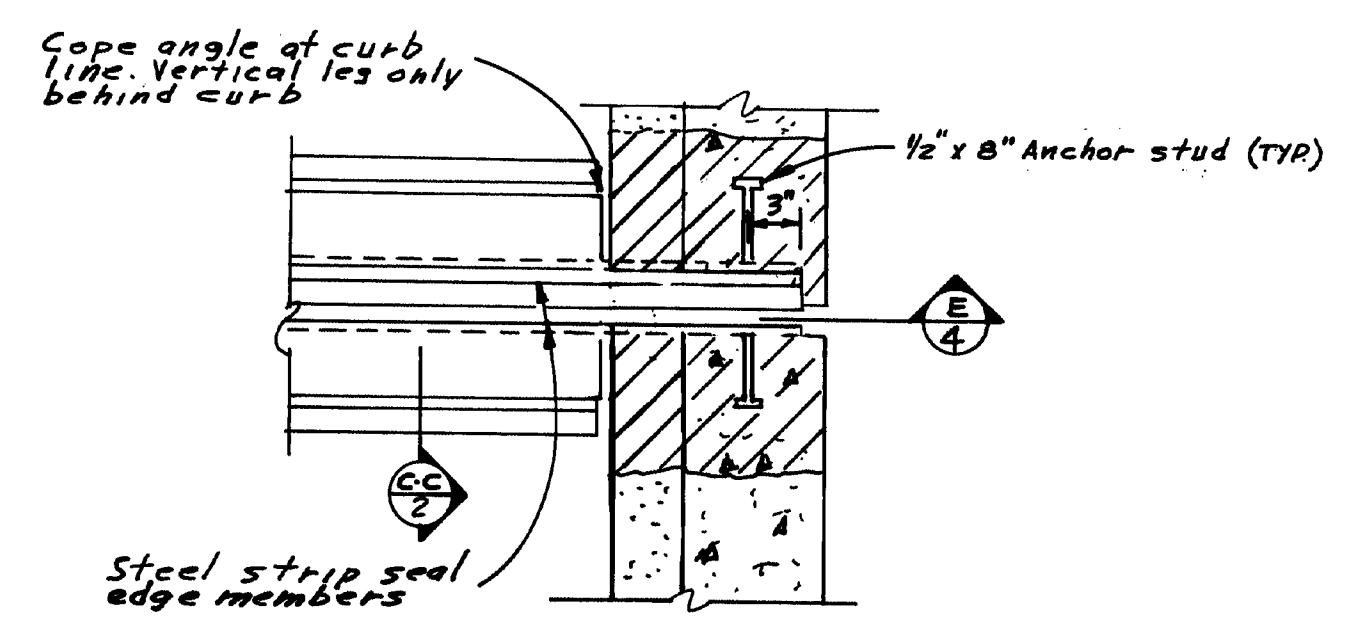
Revised at Audit Dec 1989

Rpm

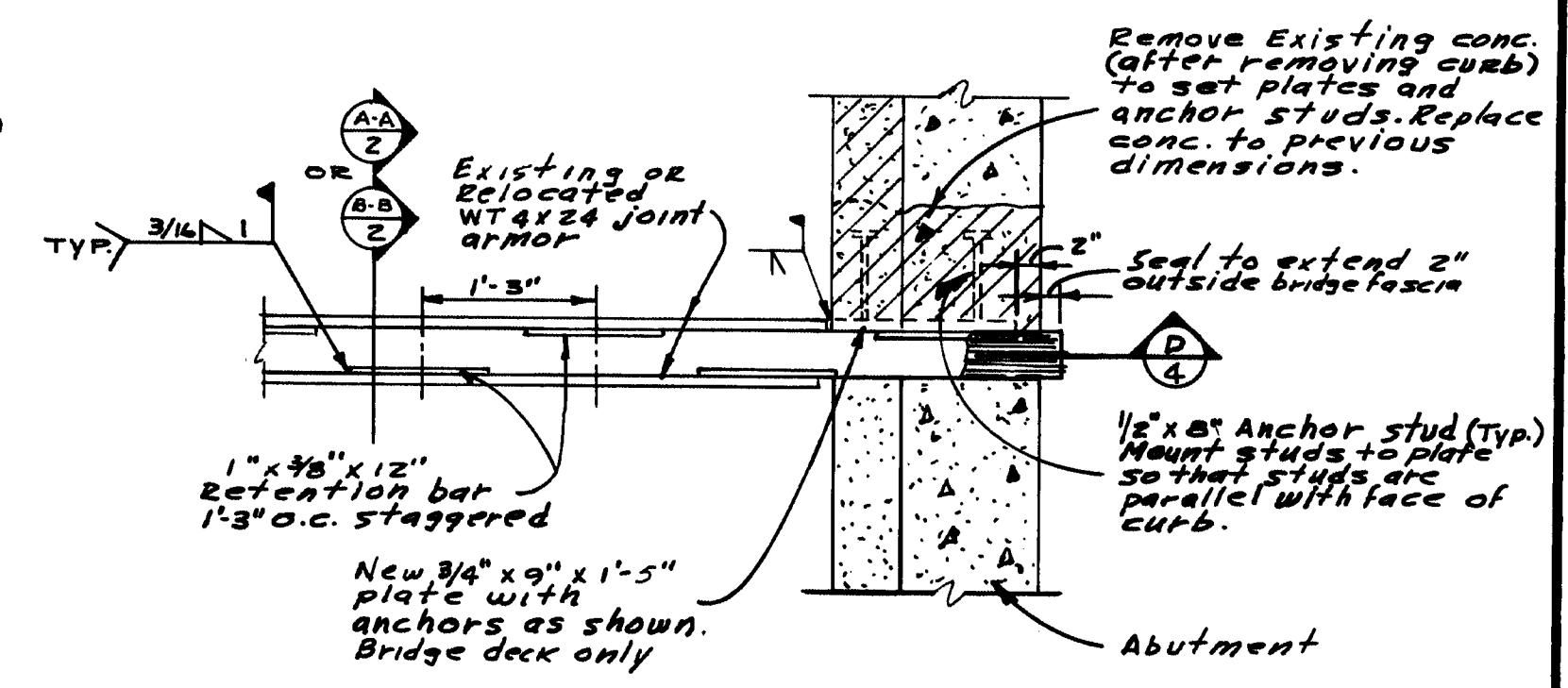
| PROJECT DESIGN ENGINEER | BY | DATE |
|-------------------------|-----|-------|
| DESIGN-DETAILED | DAK | 10-11 |
| CHECKED | BLH | |
| REVISIONS | | |
| FIELD CHANGES | | |

BRUNING 44-132 45710-1

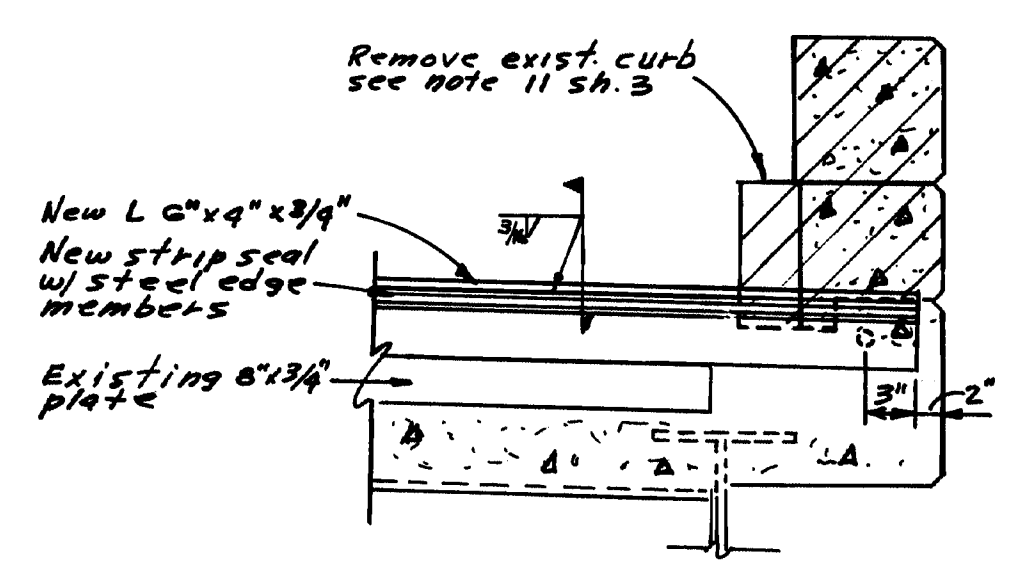
| F.R.S. NO. | STATE | PROJECT NUMBER | SHEET NO. | TOTAL SHEETS |
|------------|-------|----------------|-----------|--------------|
| 1 | MAINE | 95-7(96) | 5 | 14 |



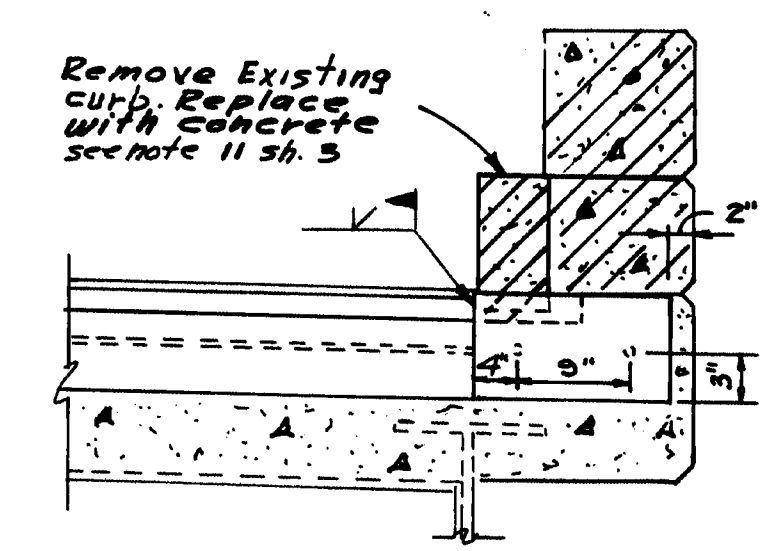
PARTIAL PLAN - GLAND SEAL
See sheet-5 for add'l details



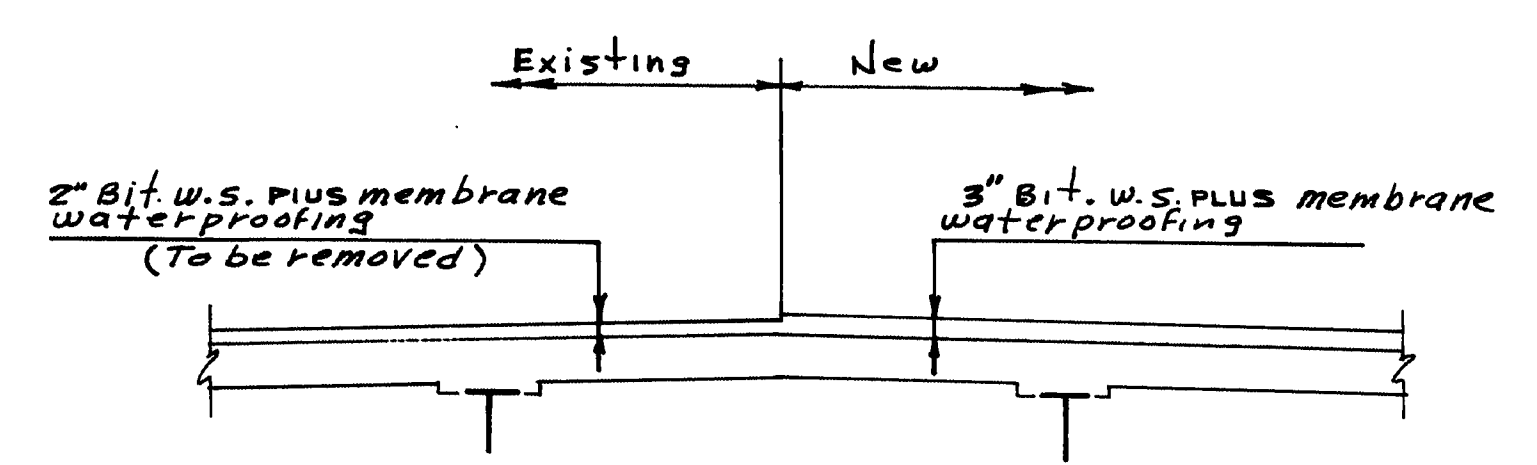
PARTIAL PLAN - COMPRESSION SEAL
See sheet-5 for add'l details



SECTION E



SECTION D



TYPICAL CROSS SECTION

| | |
|-------------------------|------|
| PROJECT DESIGN ENGINEER | DATE |
| DESIGN - DETAILED | BY |
| CHECKED | DAY |
| REVISIONS | BY |
| FIELD CHANGES | DATE |

CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

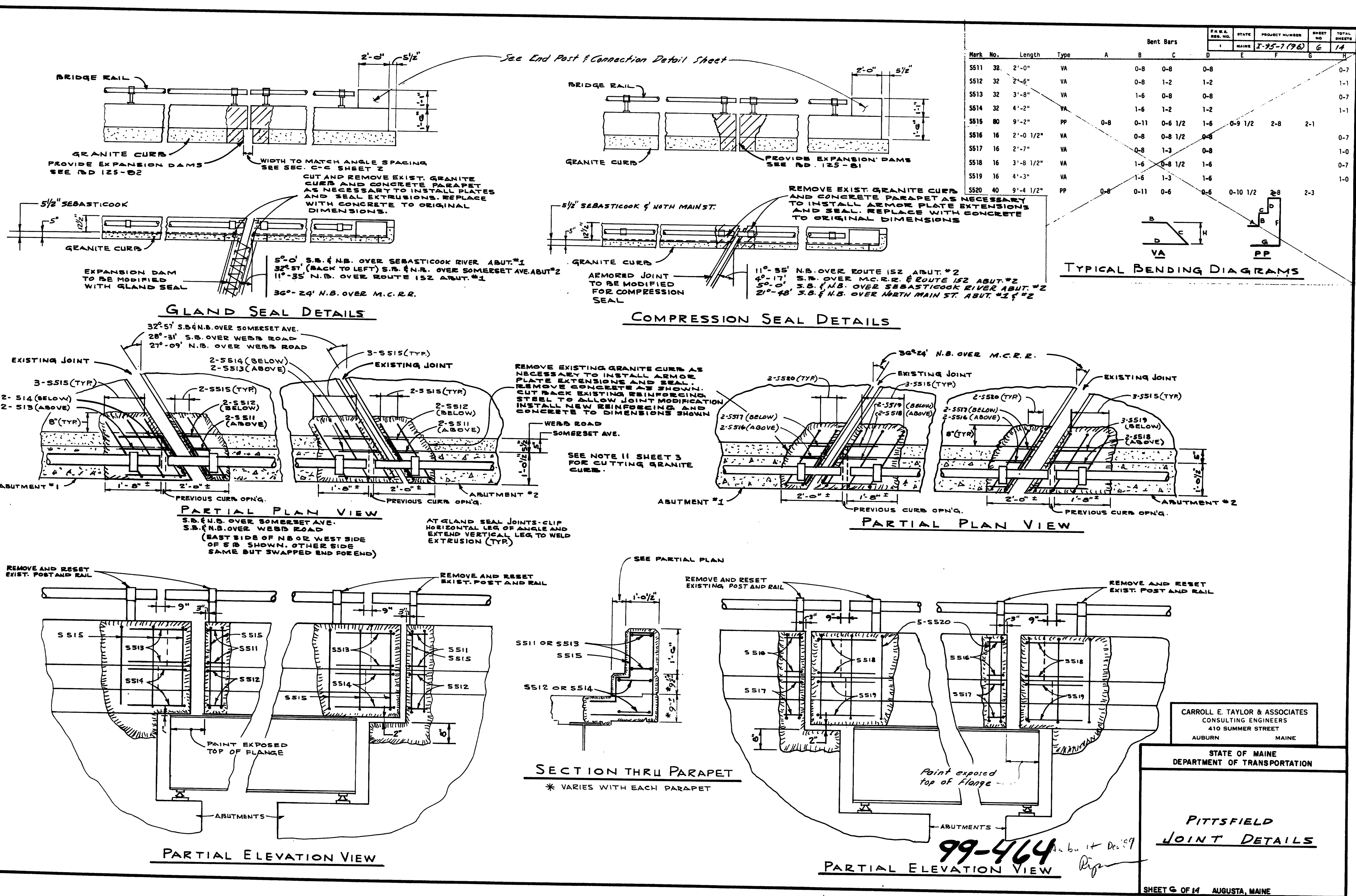
INTERSTATE 95
SOUTHBOUND AND NORTHBOUND
OVER

Sebasticook River, North Main
Street, Webb Road, Somerset
Avenue, Central Maine RR & Route 152
PITTSFIELD

SECTIONS

SHEET 5 OF 14 AUGUSTA, MAINE

99-463
As b. it Dec. 09
Rpn



| | |
|-------------------|------|
| PROJECT ENGINEER | DATE |
| DESIGN - DETAILED | BY |
| CHECKED | PAV |
| REVISIONS | BEH |
| FIELD CHANGES | |

BRUNING 44-132-45710-1

CARROLL E. TAYLOR & ASSOCIATES
CONSULTING ENGINEERS
410 SUMMER STREET
AUBURN MAINE

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

PITTSFIELD
JOINT DETAILS

SHEET 6 OF 14 AUGUSTA, MAINE

99-464

As bu 14 Dec 89
Rip

| REINFORCING STEEL SCHEDULE | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|-----|--------|----------------|------|-----|--------|----------------|------|-----|--------|----------------|------|---------------------------------------|--------|-------|----|--------|-----------|--------|-------|---------|-------|-------|---|-------|----------|--|--|--|--|
| STRAIGHT BARS | | | | | | | | | | | | | BENT BARS | | | | | | | | | | | | | | | | | |
| MARK | NO. | LENGTH | LOCATION | MARK | NO. | LENGTH | LOCATION | MARK | NO. | LENGTH | LOCATION | MARK | NO. | LENGTH | TYPE | A | B | C | D | E | F | G | H | O | R | LOCATION | | | | |
| SEBASTICOOK RIVER NB (END POST) MAINE CENTRAL RR NB (END POST) SOMERSET AVENUE NB (END POST) | | | | | | | | | | | | | TYPICAL AT ALL JOINTS (adjust to fit) | | | | | | | | | | | | | | | | | |
| H500 | 32 | 5'-4" | 8 each | H500 | 16 | 5'-4" | 8 each | H500 | 32 | 5'-4" | 8 each | | | | | | | | | | | | | | | | | | | |
| H501 | 8 | 1'-2" | 2 each | H501 | 4 | 1'-2" | 2 each | H501 | 8 | 1'-2" | 2 each | | | | | | | | | | | | | | | | | | | |
| V500 | 8 | 1'-5" | 2 each | V500 | 4 | 1'-5" | 2 each | V500 | 8 | 1'-5" | 2 each | | | | | | | | | | | | | | | | | | | |
| V501 | 1 | 1'-8" | 1 | V501 | 1 | 1'-8" | 1 | V501 | 1 | 1'-8" | 1 | | | | | | | | | | | | | | | | | | | |
| V502 | 1 | 4'-0" | 1 | V502 | 1 | 4'-0" | 1 | V502 | 1 | 4'-0" | 1 | | | | | | | | | | | | | | | | | | | |
| V503 | 8 | 4'-4" | 2 each (dowel) | V503 | 4 | 4'-4" | 2 each (dowel) | V503 | 8 | 4'-4" | 2 each (dowel) | | | | | | | | | | | | | | | | | | | |
| V504 | 16 | 3'-0" | 4 each (dowel) | V504 | 8 | 3'-0" | 4 each (dowel) | V504 | 16 | 3'-0" | 4 each (dowel) | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | 5511 | 32 | 2'-0" | VA | | 8" | 8" | 8" | | | | | | | | | | |
| | | | | | | | | | | | | | 5512 | 1 | 2'-6" | 1 | | 8" | 1'-2" | 1'-2" | | | | | 1'-1" | | | | | |
| | | | | | | | | | | | | | 5513 | 1 | 3'-8" | 1 | | 6" | 8" | 8" | | | | | 7" | | | | | |
| | | | | | | | | | | | | | 5514 | 32 | 4'-2" | VA | | 6" | 1'-2" | 1'-2" | | | | | 1'-1" | | | | | |
| | | | | | | | | | | | | | 5515 | 80 | 9'-2" | PP | 8" | 11" | 6 1/2" | 1'-5" | 9 1/2" | 2'-8" | 2'-1" | | | | | | | |
| | | | | | | | | | | | | | 5516 | 16 | 2'-1" | VA | | 8 1/2" | 8 1/2" | 8" | | | | | 7" | | | | | |
| | | | | | | | | | | | | | 5517 | 1 | 2'-7" | 1 | | 8" | 1'-3" | 8" | | | | | 1'-0" | | | | | |
| | | | | | | | | | | | | | 5518 | 1 | 3'-9" | 1 | | 1'-6 1/2" | 8 1/2" | 1'-6" | | | | | 7" | | | | | |
| | | | | | | | | | | | | | 5519 | 16 | 4'-3" | VA | | 1'-6" | 1'-3" | 1'-6" | | | | | 1'-0" | | | | | |
| | | | | | | | | | | | | | 5520 | 40 | 9'-5" | PP | 6 1/2" | 11" | 6" | 8" | 10 1/2" | 2'-8" | 2'-3" | | | | | | | |
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| FWA NO. 1 | STATE MAINE | PROJECT NUMBER [R-95-7(96)] | SHEET NO. 8 | TOTAL SHEETS 14 |
|--------------|----------------|--------------------------------|----------------|--------------------|

TYPE-BENDING DIAGRAMS

All dimensions are out to out of reinf. bar.
Bending details and hooks shall conform to the recommendations of the current revision of ACI Standard 318. Δ
Reinforcing Bar: ASTM A615 Grade 60

GENERAL NOTES

- First digit(s) following the letter of the Mark indicates size of reinf. bar.
Mark (A 502) bar size - #5
Mark (P 1001) bar size - #10
Mark (S 603) bar size - #6
- Each truss bar, Type B, may be replaced by two (2) straight bars (one top & one bottom) of the same bar size as the truss bar. Payment in either case shall be based on truss bars as scheduled on plans.

A 11 + Dec 59 *Rgn*

| | |
|-------------------------------|---------|
| Δ New Bent Bar Type 53 | 9-26-89 |
| Δ Revised ACI Standard | 5-12-83 |

REVISIONS **DATE**

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

REINFORCING STEEL SCHEDULE
PITTSFIELD
INTERSTATE 95 OVER
Sebasticoak River North Maine St.
Webb Road, Somerset Avenue,
Central Maine RR & Route 152
Northbound & Southbound

99-466

SHEET 8 OF 14 AUGUSTA, MAINE